

**TOWN OF LEMPSTER
BOARD OF SELECTMEN
Approved Meeting Minutes
February 27, 2024**

Board Present: Traci Homol, Everett Thurber and Mark Lightner

Administrative Assistant Robin Cantara

NH DOT Representatives: Lee Baronas, Amanda Joe Zatecka, Corey Spetelunas, Chris Turgeon and Bill Lambert.

Others Present: David Richards, Barbara Richards, Sherri Moen, Carrie Richards, Tom Madziarek, Mindy Chadwick, Kevin Bill, Bob Porter, Jim Cheever, Cindy Cheever, Read Gildner-Blinn, Jennifer Gildner-Blinn, Linda Callahan, Representative Judy Aron, Representative Hope Damon, Jacky Cormier, Micaiah Cormier, Wanda Ash, Racheal Tirrell, Phil Tirrell, Kevin Onnella, Scott Goodspeed, Phillip Howard, Robin Maguire, Jon Thomas and others (no sign-ins).

Mark called the meeting to order at 7:00 P.M.

The Board reviewed and approved the payroll and accounts payable manifest.

Approve/amend the minutes of the February 13, 2024 Selectmen's Meeting. Traci moves to accept the minutes as written, seconded by Everett: all in favor.

Mark thanked Jim Lewis for the use of the school and Representatives Judy Aron and Hope Damon for their support on the Route 10/Mountain Road and 2nd New Hampshire Turnpike intersection.

The meeting was turned over to Bill Lambert to discuss the State's role in the intersection of Route 10 at Mountain Road and 2nd NH Turnpike. Bill introduced the State personnel present.

Bill began his power point presentation to cover the background of the intersection, safety concerns, previous measures taken, 2- way verses all- way stops at rural intersections and the State's recommendation for reducing crashes and improving safety.

Residents voiced several concerns over visibility, terrain, safety of pedestrians and speed.

State personnel addressed the concerns of those present backing up their decision that an all-way stop is the best immediate solution to the concerns voiced; see power point presentation attached.

Bill mentioned that the desire for the Town to have crosswalks would be the Town's responsibility to maintain and the Town would need to work with Lee Baronas on that. He explained that at any controlled stop, vehicles must yield to pedestrians whether a crosswalk is there or not.

Traci announced that prior to this meeting she was against the all-way stop but has changed her mind based on the danger to children crossing the street (from and to the Library and Town Office).

Jon Thomas asked about taking the all-way stop one step further towards protecting our Town Office building and those inside with guardrails; Bill said the Town would need to work with Chris Turgeon.

Mark echoed Traci's sentiments regarding an all-way stop and feels we should try it and if the data doesn't improve we can dig deeper into other solutions.

Everett explained that he was already in favor of the all-way stop prior to the meeting.

The State will install measures notifying travelers of the new stop signs, they are thinking 90 days will be sufficient.

Bill explained that although it's not required, a letter from the Select Board approving the all-way stop is advisable.

At 8:14 PM the State completes their presentation.

Administrative and Correspondence –

The following documents were signed by the Board:

Right to Inter (2)

Pistol Permit

Work & Clark Disbursement of Funds

Meetinghouse Rental Form

Public Participation-

Jim Grenier spoke to the Board regarding disbursements to the Library at four equal payments throughout the year and the Town will still pay payroll. Jim spoke to the town attorney and State Librarian Michael York regarding appropriations for the Library.

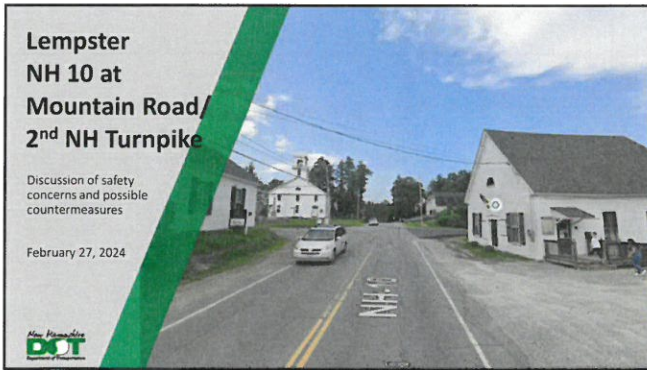
Scott Goodspeed inquired where the funds from the sale of the Long Pond lot would go; the Board explained they will be placed in the general fund.

There being no further business, Traci moved to adjourn the meeting at 8:37 PM, seconded by Mark; all in favor.

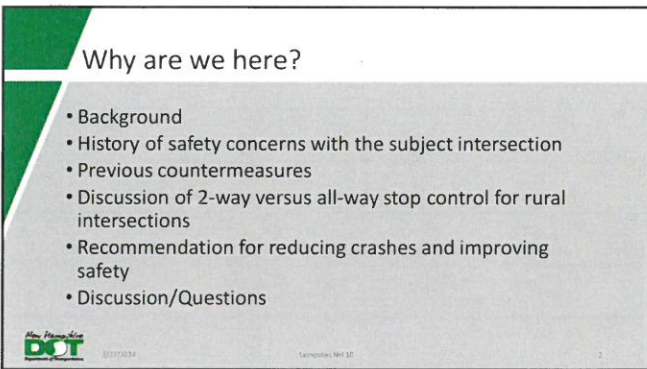
Respectfully submitted,

Robin Cantara

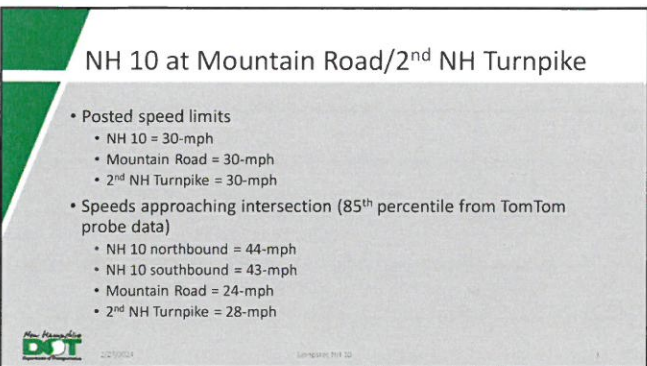
Administrative Assistant



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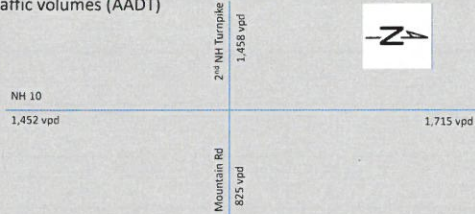
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NH 10 at Mountain Road/2nd NH Turnpike

• Traffic volumes (AADT)



2/23/2024

Lemington, NH 03055

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Safety concerns and behavior

- Speed, particularly NH 10 through the intersection
- Failure to yield
 - Mountain Road and 2nd NH Turnpike drivers failing to stop?
 - Mountain Road and 2nd NH Turnpike drivers misjudging the speed of approaching traffic?
- Pedestrian access between parking and library
- Others?



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Previous countermeasures

- Flanking STOP signs (state/town)
- STOP AHEAD warning signs (state/town)
 - Town shifted sign closer to intersection
- Flashing beacons added to STOP sign(s)(town)
- 30-mph speed limit (versus 35-mph for rural residential district, or even 55-mph for "other locations")(state)
- Brush clearing for intersection approaches and signing as needed (state/town, ongoing)



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Next Steps?


2-way versus all-way stop control?
What has changed?




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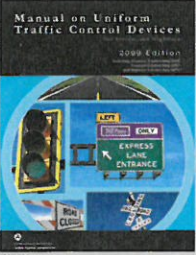
2-way versus all-way stop control

- Federal Guidelines (MUTCD)
- New Hampshire DOT past-experience, what has changed?
- Recent experience and crash reduction by other states
 - North Carolina
 - Delaware
- Recent New Hampshire success stories
 - Franconia, Gilmanton, Eaton
- Why all-way stop control is right for Lempster




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Federal guidance (MUTCD)



- Factors to consider include:
 - Traffic volumes on all approaches
 - Driver yield behavior with regard to all modes of conflicting traffic, including bicycles and pedestrians
 - Number and angle of approaches
 - Approach speeds
 - Sight distance available on each approach
 - Reported crash experience



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Federal guidance (MUTCD)

Guidance:

The decision to install multi-way stop control should be based on an engineering study.

The following criteria should be considered in the engineering study for a multi-way STOP sign installation:

A. Where traffic control signals are justified, the multi-way stop is an interim measure that can be installed quickly to control traffic while arrangements are being made for the installation of the traffic control signal.

B. Five or more reported crashes in a 12-month period that are susceptible to correction by a multi-way stop installation. Such crashes include right-turn and left-turn collisions as well as right-angle collisions.

C. Minimum volumes:

1. The vehicular volume entering the intersection from the major street approaches (total of both approaches) averages at least 300 vehicles per hour for any 8 hours of an average day; and

2. The combined vehicular, pedestrian, and bicycle volume entering the intersection from the minor street approaches (total of both approaches) averages at least 200 units per hour for the same 8 hours, with an average delay to minor-street vehicular traffic of at least 30 seconds per vehicle during the highest hour; but

3. If the 85th-percentile approach speed of the major-street traffic exceeds 40 mph, the minimum vehicular volume warrants are 70 percent of the values provided in items 1 and 2.

D. Where no single criterion is satisfied, but where Criteria B, C.1, and C.2 are all satisfied to 80 percent of the minimum values, Criterion C.3 is excluded from this condition.



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Federal Guidance (MUTCD)

Option:

Other criteria that may be considered in an engineering study include:

A. The need to control left-turn conflicts;

B. The need to control vehicle/pedestrian conflicts near locations that generate high pedestrian volumes;

C. Locations where a road user, after stopping, cannot see conflicting traffic and is not able to negotiate the intersection unless conflicting cross traffic is also required to stop; and

D. An intersection of two residential neighborhood collector (through) streets of similar design and operating characteristics where multi-way stop control would improve traffic operational characteristics of the intersection.



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New Hampshire DOT—What has changed?

- Long history of standing by the MUTCD guidance and objective traffic volume warrants
- “The best control is the least control”
- Past practice resulting in a growing list of intersections with elevated traffic control elements to “improve STOP recognition, compliance but where crashes continue to occur
 - Recent changes to all-way stop at intersections where “warrants” were not met
- National recognition given to two states for converting 2-way stops to all-way stops



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North Carolina

Safety Treatments - 2-Ln Minor Road Stop

- All Way Stop
- Roundabouts
- Vehicle Entering When Flashing
- Enhanced Intersection Signage
- Stop Ahead Pavement Markings
- Overhead Flashing Beacon

Volume Thresholds

General Rules of Thumb Regarding Volumes (2 lane at 2 lane intersections):

- Major Road AADT 15,000: Full AD for all roadways - One lane roundabout
- Major Road AADT 7,500: Full AD for all roadways - Full AD
- Major Road AADT 4,000: Full AD for all roadways - Full AD

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North Carolina

- Study Findings
 - 68% Reduction in Total Crashes
 - 77% Reduction in Fatal and Injury Crashes
 - 75% Reduction in Frontal Impact Crashes
- Benefits
 - Low cost (\$20,000 per intersection)
 - Benefit to Cost ratio of 83:1
- All-way stop control can be installed on primary routes without violating driver expectations or creating safety concerns.

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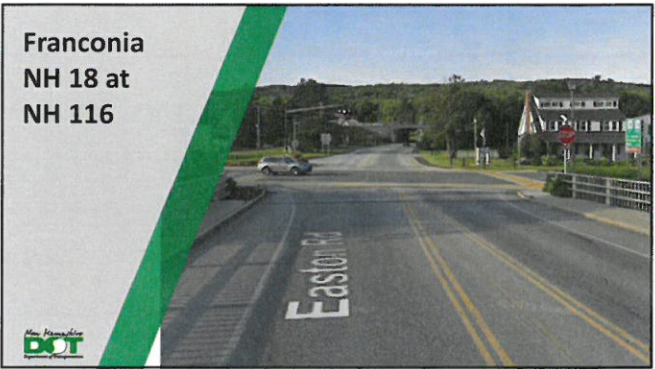
Recent NH Successes

- Franconia, NH 18 at NH 116
 - NH 116 approach crosses over a bridge which may restrict sight lines (or extend sight lines to the nearby intersection with I-93 ramps)
 - Overhead beacon, flashing beacon on STOP sign had not mitigated crashes
- Gilmanton, NH 107 at NH 140
 - Flashing beacons on STOP AHEAD signs did not mitigate crashes
 - NH 107 speeds likely exceeded posted speed limit
- Eaton, NH 153 at Ridge Road and Glines Hill Road
 - NH 153 turns 90 degrees at the intersection, stop control is for the two Class V roads

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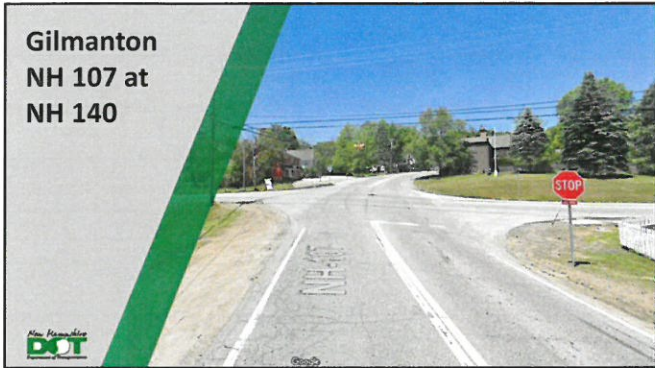
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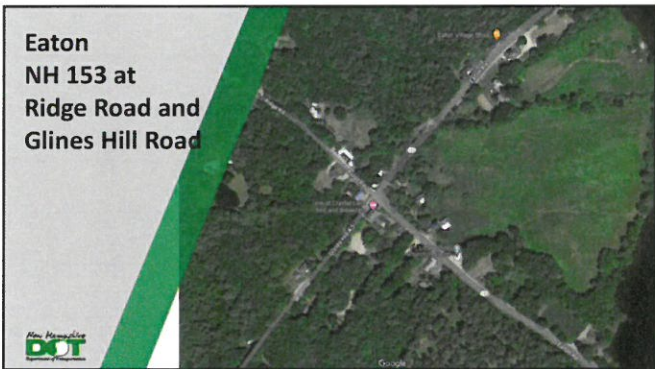
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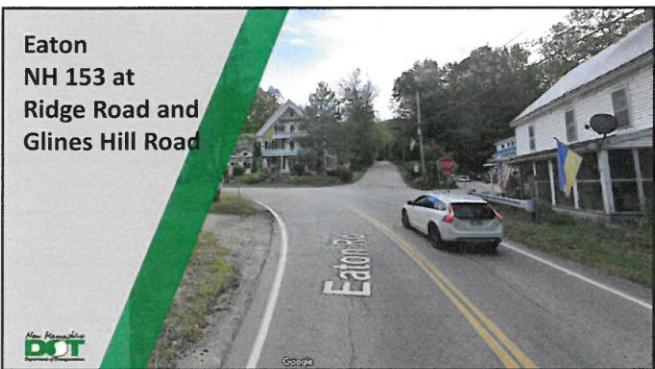
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NH
DOT
New Hampshire
Department of Transportation

NHDOT recommended change

All-way stop control

• Pros

- Promotes safer speeds on NH 10
- Requires all drivers to stop, reduces odds of two drivers entering intersection at the same time
- Slower/stopped traffic provides a safer environment for pedestrians
- Does not require long-term commitment to flashing beacons or other STOP sign enhancements
- Can be implemented almost immediately

• Cons

- Requires short-term re-education of familiar drivers (enhanced conspicuity for new STOP signs and short-term deployment of changeable message signs)
- Introduces minor delay for NH 10 traffic

2/2/2024

Lampeter, NH 10

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Questions/Comments



8/27/2024

Lampeter, NH 10

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